

"A" cannot sail below her normal course since "B" has overlapped.

"A" cannot sail below her normal course if "B" is attempting to pass and is within 3 lengths of "A."

V. Award of Prizes

1. Prizes shall be awarded on the basis of the total number of points earned for the series minus any penalties imposed by the Race Committee for infractions of rules.

VI. Penguin Scoring

1. A boat shall earn points for each race in accordance with the following:
 - (a) One point for finishing.
 - (b) One point for each boat defeated; boats making a proper start and not finishing are considered as boats defeated.
 - (c) A bonus of seven-tenths of a point for finishing first.
 - (d) A bonus of three-tenths of a point for finishing second.
 - (e) A bonus of one-tenth of a point for finishing third.
2. Only boats making a proper start in at least two-thirds of the races shall be considered as racing insofar as points score is concerned.
3. In cases of ties, the boat which defeats the other the greatest number of times shall be declared the winner. If the tie still exists the boat with the lowest elapsed time for the races in which both boats finish, shall be declared the winner.

VII. Infraction of Rules

1. A contestant desiring to call attention to infractions of the rules must notify the Race Committee of such infraction immediately upon finishing the race, unless unusual circumstances prevent such action. In such case the Race Committee shall be notified as soon as possible.

VIII. Penalty for Infraction of Rules

1. No penalty shall be assessed until a hearing is held and the contestants involved in the infraction of the rules shall be notified and shall have the right to be present at such hearing.
2. The Race Committee shall assess penalties for infraction of rules. The penalties shall consist of points to be deducted from the total points earned by a boat. The penalty shall be not less than that given below for the class of rule infraction involved, and if in the judgment of the Committee the best

interest of the sport would be served, the penalty may be increased by an amount up to the total number of points earned by a boat in any one race.

3. Class I violation of the rules consists of violations of the rules in which a boat, while violating a rule does not interfere with or put at a disadvantage any other contestant. If a boat is deemed guilty of a Class I infraction, not less than two points, or twenty per cent of the points of first place not including bonus points, whichever is the greater, shall be assessed.

4. Class II violation of the rules consists of violations wherein boat touch or rights are not granted or interferences occur but in which the advantage to the other boat is negligible or in which the boat having the rights does not change in position in the race at that point and due to that infraction of the rules. If a boat is deemed guilty of a Class II infraction, not less than two points, or forty per cent of the points of first place not including bonus points whichever is the greater, shall be assessed.

5. Class III violations of the rules consist of violations wherein collision occurs, rights are not granted or interferences occur which result in a major disadvantage to the boat holding rights or in which the boat holding rights loses position in the race at that time due to that infraction. If a boat is deemed guilty of a Class III infraction, not less than four points, or eighty per cent of the points of first place not including bonus points, whichever is the greater shall be assessed.

6. A boat found guilty of the infraction of any rule shall forfeit bonus points of first, second or third places.

7. In cases of collision which disables a boat so that it becomes necessary to abandon the race, the boat responsible of violating a rule shall receive a penalty of all points earned in that race.

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Instead of a bailing tin can use a paper milk carton as it does not scratch the varnish.

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General Notes

The subject of Racing Rules is one that all racing skippers should master, for it is a major part of the sport of yacht racing. Only too often a first class helmsman, who knows the last word in racing tactics, is penalized for lack of knowledge of a racing rule. It is suggested that every Fleet hold a "Skipper's Meeting" to discuss the changes in the rules at the start of the season, and later as questions of interpretations of the rules arise.

It is suggested that all skippers obtain copies of the N.A.Y.R.U. Racing Rules for reference purposes. Copies may be obtained from the Corresponding Secretary, 37 West 44th Street, New York 18, N. Y., for 50 cents a copy. Appendix I contains "The Proprieties of Yacht Racing" which are obligations to be observed by all racing skippers and Appendix II contains the "Govern-

ment Right of Way Rules" which are observed when not racing and at all times between other yachts. Other parts are applicable when participating in regattas sailed under N.A.Y.R.U. Rules.

As heretofore under the Penguin Rule, an infringement of the rules does not cause disqualification. A penalty of loss of points is assessed for each infringement, the number determined by the class of violation. It is therefore imperative for each skipper who knowingly violates a rule to report his action to the Race Committee at the conclusion of the race, rather than to withdraw. It also follows that protests should not result from known rule violations but only from questions of interpretation of the rules. In lieu of flying the protest flag, the protesting boat must notify the boat protested.

It is the duty of both crews involved in a protest to present clear statements of the situation in question to the Race Committee. It is only when this is done that the Race Committee is able to intelligently interpret the rules and decide whether the protest is to be upheld or disallowed.

The fact that a boat is not among the leaders in a race does not obviate the requirement that a violation be reported or protest filed, as has been mistakenly assumed by some skippers. Each boat is racing against each of the others and each skipper is entitled to both his rights and his obligations whether he be finishing first or last. It is likewise an obligation of the Race Committee to settle every protest equitably and promptly.

With reference to deduction of points assessed for Class I, II, or III violations, penalties are assessed only against points earned in the race in question. A minimum penalty is based on a percentage of the points of first place not including bonus points; a penalty may include all the points earned in the race (disqualification) but no points earned in another race may be deducted.

Fleet Charters

The Executive Vice President issues new Fleet Charters in accordance with Article VII, Section 2, of the Constitution, and other considerations set forth herein. The first prerequisite is a fleet organization. The minimum requirement calls for three Active Members of the P.C.D.A.—that is, **paid-up** members owning measured boats. However, a fleet of three boats is only a start—fleets do not develop successfully unless there are more potential members, boats under construction or other plans for progressive expansion. Another quite vital element is the sponsorship or backing of a yacht club or other responsible organization—not an absolute necessity, but it is a big help to have a home port and a race committee to call upon.

When presenting a request for a Fleet Charter, it is necessary to include: proposed fleet name, home port, names of fleet officers and roster of boats and members. The latter should include: name of member, address of member, number of boat and name of boat. If there are Associate Members of the P.C.D.A. in the fleet, their names and addresses should be included on a separate list. Forwarding this information in duplicate will enable the Executive Vice President to send one copy to the Secretary for his records.

The name of the Fleet should be of purely local significance, omitting the words "Yacht Club". In most cases, the name of the body of water on which the fleet sails or its home port should be used.

FLEET REPORTS—PENALTIES

We quote from Article VII, Section 3, of the Constitution:

"Each fleet shall be required to file an annual report of its activities and a complete roster of fleet officers and members with the Secretary of the Association, during the month of January of each year, in order to renew its rights and privileges under its charter. Charters may be suspended at any time by the Executive Committee at its discretion, but can only be revoked at the annual meeting for failure to maintain in good standing the minimum of three boats necessary for a new fleet, or for adequate reasons."

The terrific job of compiling the material and arranging for early publication of the Year Book should not be further complicated by difficulties in receiving all fleet reports promptly. Most fleet officers have done a good job—but some have had to be coaxed. It should not be necessary to write numerous letters and send telegrams to get these reports together.

In the future, the above shall be strictly enforced. We urge you to do all you can to keep your fleet active in the organization by electing officers who are on the job—interested in fleet and association activities—and who will report promptly all fleet news, whether there is much or little activity.

Procedure for Chartering School, College, or Other Organization Fleets

A—Dinghys Owned by Sailing Club

1. Dinghys shall be registered in the name of the Club. Dues for one Active Membership shall be paid by the Club.
2. Properly qualified Racing Skippers of the Club shall pay dues and be listed as Active Members (co-owners) by virtue of their membership in the Club. Skippers in sanctioned events shall be Active Members.
3. Members of the Club who do not qualify as Racing Skippers, who crew in sanctioned events, shall pay dues and be listed as Associate Members.
4. Dues shall be optional for other members of the Club, who may be Associate Members if desired.
5. Fleet Charter shall be issued to the (name of school, college or organization) Penguin Fleet upon the registration of three or more dinghys and Active Members. Fleet Officers shall normally be elected from among the Active Members of the Fleet. Membership shall be restricted to members of the Club who are Active or Associate Members of the International Penguin Class Dinghy Association.

B—Dinghys Owned by Individuals of a Sailing Club

1. Dinghys shall be registered in the name of the individual owner or co-owners, who shall pay dues as regular Active Members. Skippers in sanctioned events shall be Active Members.
2. (Same as A-3, see above, substitute "Active Member" for "Racing Skippers.")
3. (Same as A-4, see above.)
4. (Same as A-5, see above.)

Notes: 1—Fleets organized as outlined above and having Active and Associate Members for Skippers and Crews in sanctioned events will thereby fulfill all the pertinent requirements of the Constitution and By-Laws of the Penguin Class Dinghy Association.

2—Fleets of the Club owned dinghys (Type A organization) are particularly desirable. Where dinghys are owned by individuals (Type B), unless these are permanent residents of the school or college, the Fleet will not have the permanent character of the former type.

Measurement Regulations

1. Measurements shall be made by the owner, builder, or a measurer of a Yacht Club.
2. This measurement sheet shall be forwarded in duplicate to the Chairman, International Measurement Committee, who shall approve or reject the boat as a class member accordingly if boat is within or beyond the building tolerances established by the ruling committee.

The registration of your boat is not complete until your Measurement Certificate has been approved by the International Measurement Committee. Only measured boats are permitted to race in the sanctioned events.

3. Boats shall be subject to re-measurement as to dimensions and data on the measurement sheet upon protest. The person protesting shall post a bond of \$2.00 as evidence of good faith, which shall be used to pay the measurer if the measurements are in accordance with those on the boat's measurement sheet. If dimensions fail to check with those submitted on the measurement sheet, the bond shall be returned and owner shall pay re-measurement fee, which shall not exceed \$2.00. Re-measurement shall be made by the measurer appointed by the Race Committee having control of the races.

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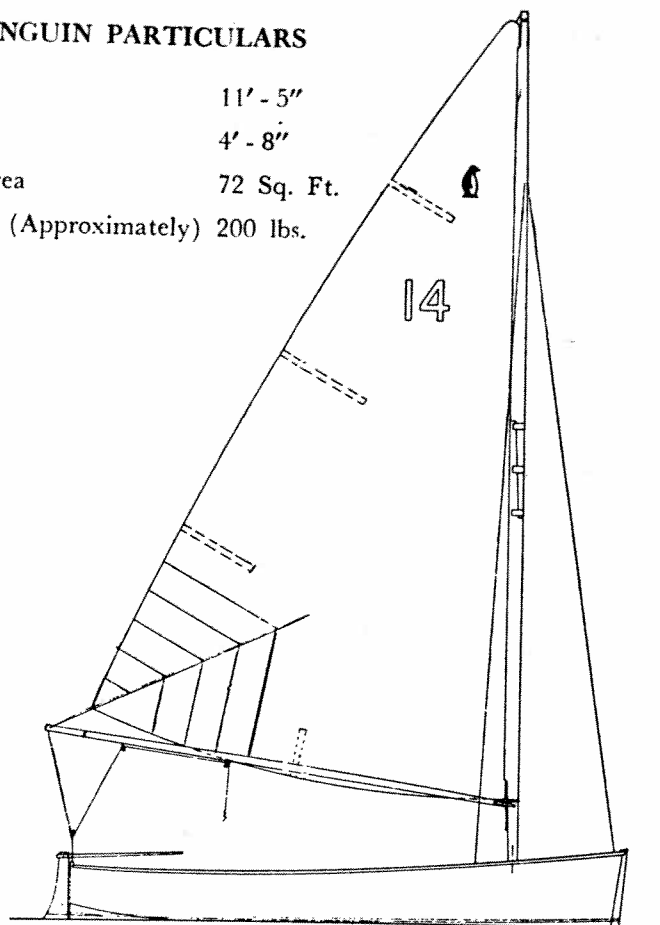
Procedure for Measuring Penguin Dinghy

1. With the boat right side up, measure its length from stem head to after side of transom (see measurement sheet).
2. Divide this measured length by 4.
3. Measure from the stern and mark with chalk, the $\frac{1}{4}$, $\frac{1}{2}$ and $\frac{3}{4}$ length of the boat. (Sections A, B, C, D, on sketch).
4. Measure the beam of the boat (dimension Q) at sections A, B, C, D.
5. Turn the boat upside down and stretch tightly a chalk line (hard cotton string) located over the center of the boat and six inches above the stem and transom (see measurement sheet). This is most easily accomplished by setting a light post up forward and aft, and adjusting the string to the height and centerline specified above.
6. Mark sections A, B, and C on the boat by using a flexible batten and chalk or by snapping a chalk line. (This is done by running a hard cotton line through chalk, stretching it tightly over the section to be marked, raising the center line and letting it snap back onto the section.)
7. Make the measurements described on the measurement sheet in duplicate.

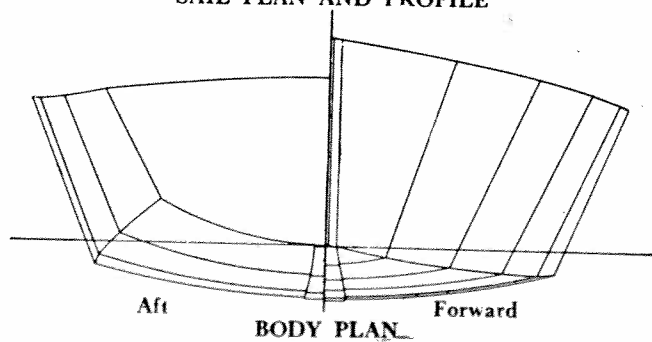
8. Upon completion send to the class measurer, Monte Clare, 2191 Mesa Drive, Santa Ana, Calif.

PENGUIN PARTICULARS

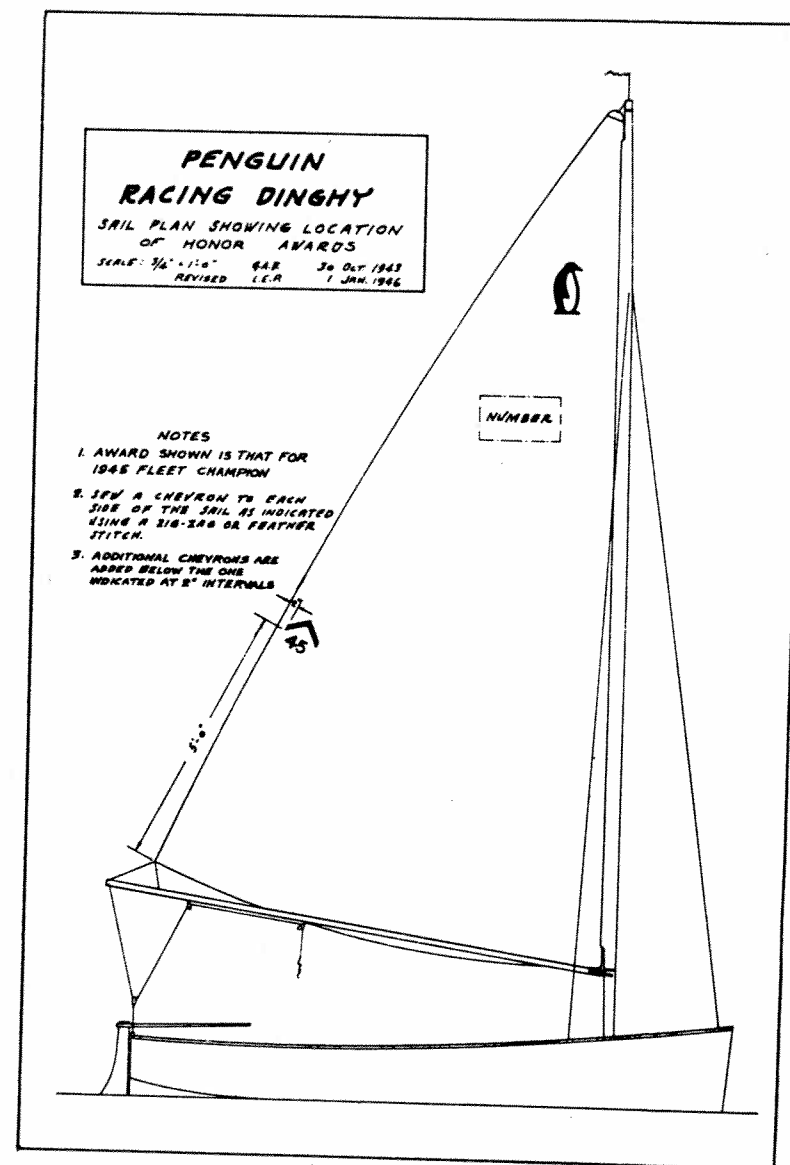
Length 11' - 5"
 Beam 4' - 8"
 Sail Area 72 Sq. Ft.
 Weight (Approximately) 200 lbs.



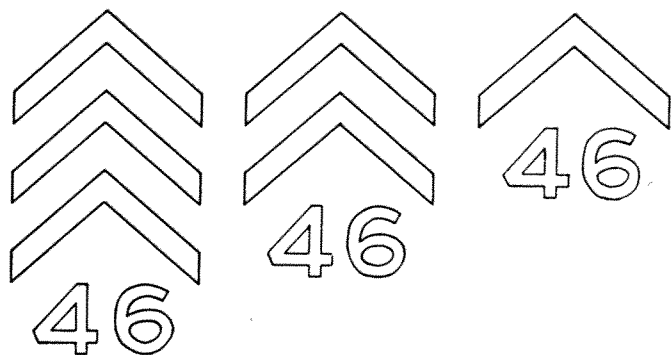
SAIL PLAN AND PROFILE



BODY PLAN



HONOR AWARDS



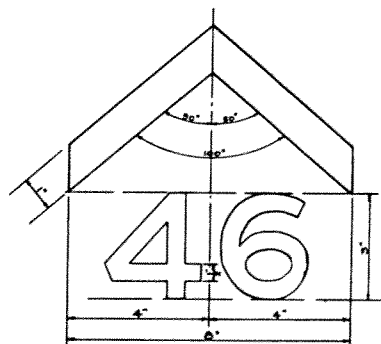
FIRST PLACE

SECOND PLACE

THIRD PLACE

CHEVRONS AND YEAR IN GOLD FOR CLASS A EVENT SUCH AS NATIONAL CHAMPIONSHIP
 " " " " BLUE " " B " " INTER REGIONAL CHAMPIONSHIP
 " " " " RED " " C " " REGIONAL CHAMPIONSHIP
 CHEVRON AND YEAR IN BLACK " " D " " FLEET CHAMPIONSHIP

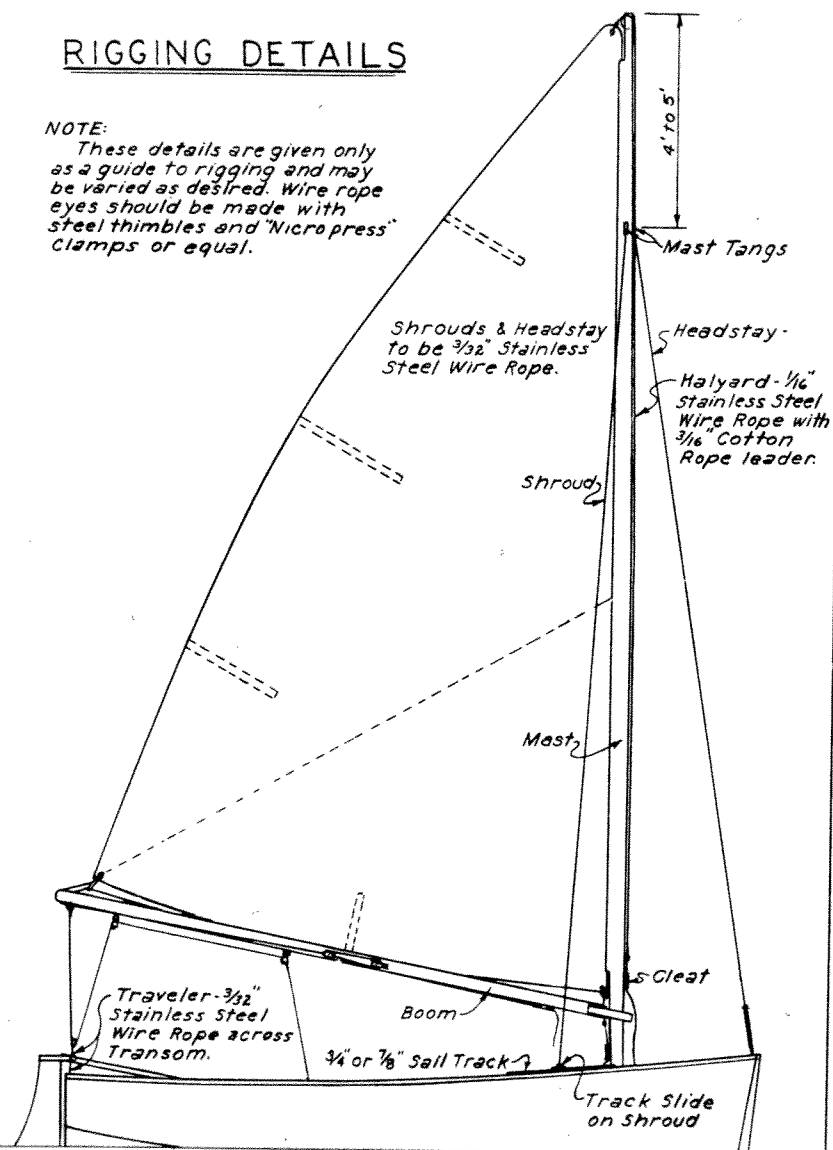
DETAIL OF CHEVRON AND NUMBER

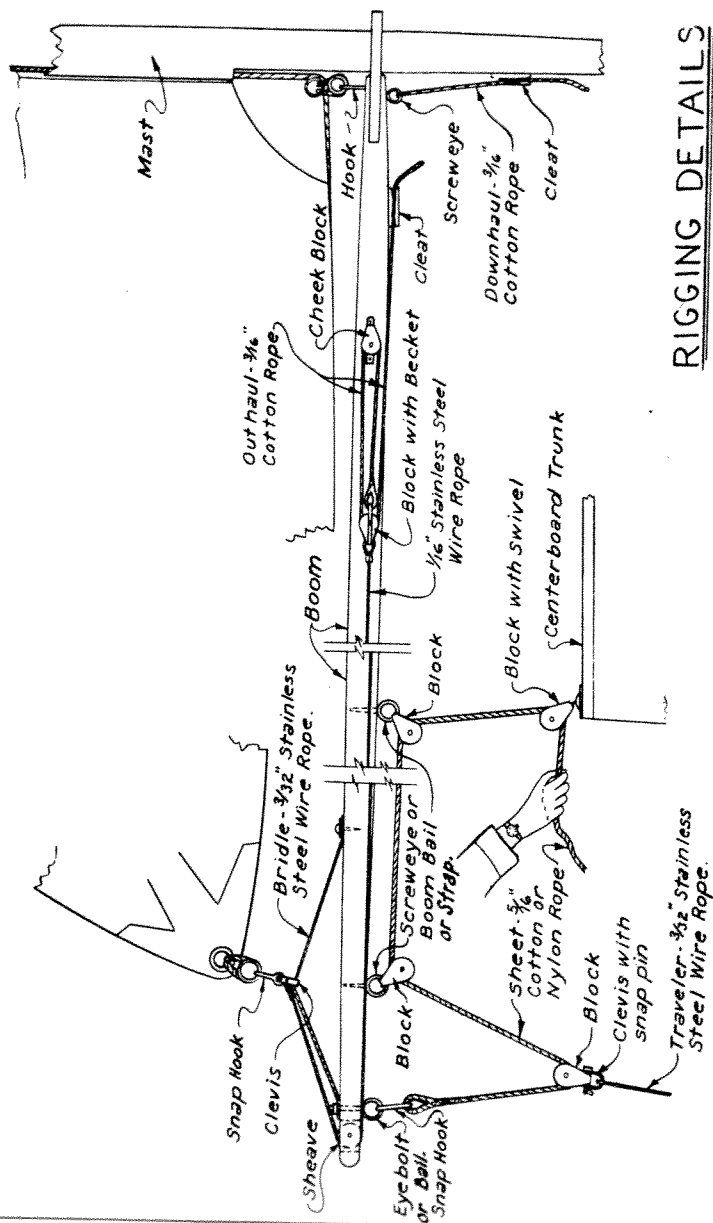


RIGGING DETAILS

NOTE:

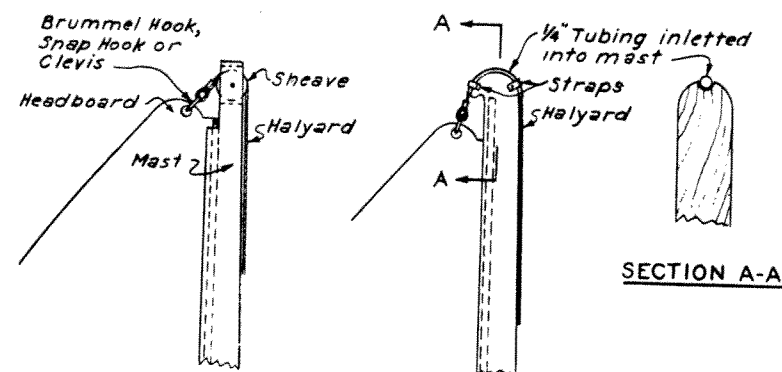
These details are given only as a guide to rigging and may be varied as desired. Wire rope eyes should be made with steel thimbles and 'Micropress' clamps or equal.



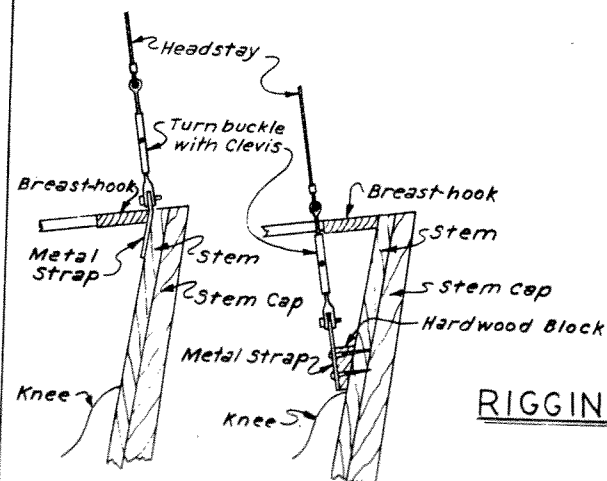
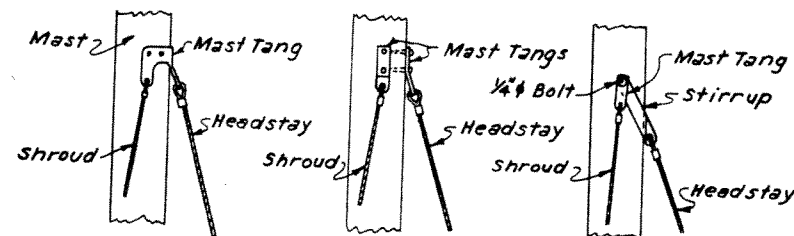


RIGGING DETAILS

SHEET 2 OF 3



SECTION A-A



RIGGING DETAILS

SHEET 3 OF 3