

- (b) Skipper (Helmsman) must: (1) Be an active member, be enrolled in the fleet he represents, be a bona fide owner, or designated syndicate representative of boat to be sailed by him. (2) Have sailed his own boat in a representative number of Penguin races within a year, and by reason of residence or normal yachting activities be qualified to represent that locality. Requirement (2) may be modified by the Executive Committee for the newly formed fleets and service fleets.
- (c) Crew: It is recommended that the crew be an associate or active member enrolled in the fleet he represents, come from, and be qualified to represent that locality.
- (d) No changes shall be made in the skippers or crews during the International Regatta, except in cases of serious illness, accident or other cause satisfactory to, and approved by, the Race Committee.

8. *Protests and Hearings*

- (a) The Penguin Racing Rules as published in this Book shall apply.
- (b) A contestant desiring to call attention to infractions of the rules must notify the Race Committee of such infraction immediately upon finishing the race, unless unusual circumstances prevent such action. In such cases the Race Committee shall be notified as soon as possible.
- (c) Code Flag B need not be displayed and protests need not be made in writing, except as directed by the Race Committee in order to determine more accurately the facts of the protest.
- (d) Hearings shall be held as soon as practicable and before the ensuing race if possible. Penalties for infractions have been made flexible in order to make possible fair and just penalties for infractions as it is not believed that all infractions of a minor nature justify disqualification.

9. *Notices*—Official notice of hearings, decisions, rulings, changes in procedure, and instructions shall be served upon all parties by posting on bulletin board at headquarters daily during series. By due notice, time and place may be varied because of conflicting conditions. Responsibility of being at Headquarters and seeing notices at given hours rests with contestants and sub-officials.

10. *Points*—Scoring shall be in accordance with Section VI of the Penguin Racing Rules.

11. *General Procedure*

- (a) All instructions and regulations not fully described in the International Championship Rules shall be published in the Race Circular.
- (b) A special Race Committee, known as the International Race Committee (I.R.C.) shall be appointed by the Executive Committee to supervise and conduct the carrying out of the International Regatta. It may consist of the members of the Executive Committee that can be present and eligible to serve by reason of not being contestants. The Chairman of the local Regatta Committee shall be a member of the International Race Committee. Such additional members as may be necessary to make the International Race Committee an odd number of at least five shall be appointed.
- (c) The International Race Committee shall have supervision of the selection and laying out of the course and general conduct of the Regatta. All other details in connection with the holding of the International Regatta, such as place, shall be submitted to the Executive Committee for approval and sanction.
- (d) The decision of the International Race Committee as to selection of course and conduct of Regatta shall be final and from the period of midnight of the day preceding the scheduled first race of the Regatta until 24 hours after the last race, the Executive Committee shall be inactive and the decisions of the I.R.C. as to matters concerning the current Regatta shall be final, but shall not extend to other matters nor revoke the Executive Committee Rulings.
- (e) Timers, starters, and other officials may be appointed from the local Regatta Committee by the I.R.C.

ARTICLE X—*Specifications*

1. These specifications describe the construction of the Penguin Class Dinghy and must be adhered to. The Official Plans show an approved method of construction as to details and sizes; however, these specifications give the minimum sizes which are allowed. The materials of construction are optional except as specified. The sizes of members are optional except as specified. WHERE ANY POINT OF DIFFERENCE EXISTS, THE SPECIFICATIONS AS WRITTEN IN THE YEARBOOK SHALL BE FINAL, AND NOT THE PICTURED DESCRIPTION AS SHOWN IN THE PLANS. To qualify in sanctioned races, a Penguin Dinghy must conform to these specifications.

Try using tiny safety pins to hold one-half inch wind flys on your stays. They don't foul up so much as the narrow ribbons.

It will help the Race Committee if your boat is named and not just a number.

2. *General*—The dimensions and shape of the hull shall be in accordance with the official plans. The boat and all of its parts, except as otherwise specified, shall be constructed of wood with a weight of not less than 20 pounds per cubic foot at 15 per cent moisture content. Suggested woods are not a requirement but it is recommended that the lighter woods such as spruce, cedar, African mahogany, and fir be utilized. All plywood shall be phenolic resin bonded hot pressed waterproof plywood. THE COMPLETELY FINISHED HULL, INCLUDING CENTERBOARD, SHALL NOT WEIGH LESS THAN 130 POUNDS.

3. *Keel*—Keel may be made of one piece or two pieces, which shall have a total molded dimension of not less than $\frac{7}{8}$ " and a total sided dimension of not less than $5\frac{3}{4}$ " in way of the centerboard trunk and tapered at the ends. The tapering shall be in such a manner that at each station the half breadths of the keel on the inside of the boat shall be not less than those distances shown by the table of offsets for the half breadths of the rabbet line plus $\frac{7}{8}$ ". (Example: Width of the keel on the inside of the boat at station 6 equals $2(1\frac{1}{16} + \frac{7}{8}) = 5\frac{1}{8}$). However, the keel may be $\frac{5}{8}$ " thick if it is completely covered by the plywood. The width of the keel at the bow shall be not less than that shown for the width of the stem piece.

4. *Stem*—Stem may be in one piece or two pieces. The size inside the rabbet shall be not less than that shown on the full sized plan. The stem cap may be molded to not less than $\frac{1}{4}$ " radius.

5. *Transom*—The transom may be made of plywood or solid lumber. If made of plywood it shall have a thickness of not less than $\frac{1}{4}$ " and shall have framing not less than $\frac{3}{8}$ "x2" completely around the edges. Plywood transoms in addition to this framing shall have a center stiffener of not less than $\frac{5}{8}$ " x 4". If the transom is made of solid lumber, the thickness shall be not less than $\frac{5}{8}$ ".

6. *Knees*—The stem and transom shall be adequately secured to the keel by means of knees. The knees shall be sided not less than $\frac{3}{4}$ " and shall be of the outline shown on the full size plan.

7. *Chines*—The boat shall be fitted with two chines. The chines may be in one piece of rabbeted construction, two pieces as indicated on the plans, or a single piece entirely inside the planking. The chines shall be not less than $\frac{3}{4}$ "x $1\frac{1}{2}$ " inside the planking. The exterior radius of the chine shall not exceed $\frac{1}{2}$ ". Chines may be beveled only to the extent necessary to allow draining when the boat is on an even keel provided they are not less than 2" in width.

8. *Frames*—Frames shall be fitted in number and location as indicated in the official plans. All frames shall be sided not less than $\frac{5}{8}$ " their full length exclusive of gussets. Side frames Nos. 1 and 2 shall be a full 2" their full length; other frames shall be not less than 2" at the chine with a straight taper to not less than $1\frac{1}{2}$ " at the head. The inwale may be omitted, if so the top of the frames on the inboard side may be rounded to not more than a 1" radius. Bottom frames shall be molded not less than 2" over the top of the keel, tapered to not less than $1\frac{1}{2}$ " at the chines. Side frames and bottom frames shall be connected by plywood gussets not less than 4" on each leg and material not less than $\frac{1}{4}$ " thick. If a lap joint is used to connect the frames

there shall be one gusset placed between the side frames and the bottom frames, if a butt joint is used there shall be two gussets, one placed on each side of the joint. The inboard edge of the gussets may be rounded from end to end to a smooth curve. DISTANCE FROM INSIDE TOP CORNER OF CHINE TO NEAREST POINT ON RADIUS OF GUSSET SHALL BE NOT LESS THAN $1\frac{1}{2}$ INCHES.

9. *Gunwale*—A gunwale shall be fitted and shall be not less than $\frac{5}{8}$ "x1". When a gunwale is fitted as a rail cap, or a rail cap is fitted, its width inboard of the inside of the planking shall not exceed $1\frac{1}{2}$ " measured perpendicular to the planking. Reinforcing or track rail between frames 1 and 3 is not considered part of the gunwale. Oarlock sockets shall be installed.

10. *Fender*—A fender of not less than $\frac{1}{2}$ "x1" shall be fitted, maximum width shall not be more than 2".

11. *Thwart*—A thwart of not less than $\frac{3}{4}$ "x $5\frac{3}{4}$ " (or equivalent cross-sectional area) shall be fitted. The thwart shall be located between stations 3 and 5, and attached to the centerboard trunk at least 10" from the top of the keel.

12. *Mast Partner*—A mast partner not less than $\frac{3}{4}$ "x $5\frac{3}{4}$ " (or equivalent cross-sectional area) shall be fitted. This X-sectional limitation shall be adhered to in way of the opening for the mast. The mast partner shall be located as illustrated in the plans and shall be at least 14" above the top of the keel.

13. *Breasthook and Knees*—The gunwale shall be attached to the stem by means of a breasthook and to the transom by means of knees all being not less than $\frac{3}{4}$ " thick and not less than 6" on each leg.

14. *Planking*—Side and bottom planking shall be $\frac{1}{4}$ " phenolic resin bonded hot pressed waterproof plywood.

15. *Centerboard Logs*—Centerboard logs shall be sided not less than $\frac{3}{4}$ " and shall be molded not less than shown on the full size plan.

15 (a). *Centerboard Trunk*—Centerboard trunk sides shall be not less than $\frac{1}{4}$ " plywood and shall extend vertically a sufficient height to completely house the centerboard.

16. *Centerboard*—The centerboard shall be of wood except for loading which may be added to overcome flotation, in which case the negative buoyancy shall not exceed 2 pounds. The outline shall be that shown on the official plans. The thickness shall be not less than $\frac{3}{4}$ ". The centerboard may be streamlined except for that portion which remains above the bottom of the keel when the board is down 90 degrees, and may be controlled by a lever or other method as long as the axis about which it pivots is fixed at the point shown on the plans. No method of filling or covering the bottom of the centerboard trunk is allowed. The top of the centerboard trunk may be enclosed when a centerboard actuating mechanism is used.

17. *Rudder*—The rudder outline shall be in accordance with the rudder plan effective 1 January 1950 except that arrangements may be made to raise the rudder blade to facilitate beaching. However, during a race the adjustable rudder blade shall be carried in the position as shown on the plan. The thick-

ness of the rudder must be as specified along line "A-A" of the plan and above the line shown for the lower edge of the cheek pieces. The specification for the thickness is as follows: "THE RUDDER MAY BE $\frac{3}{4}$ " THICK IF FITTED WITH ADEQUATE CHEEK PIECES MADE OF WOOD, OTHERWISE IT SHALL BE NOT LESS THAN $\frac{7}{8}$ " THICK. THE WOOD CHEEK PIECES SHOWN ON PLAN ARE THE MINIMUM IN THICKNESS AND EXTENT THAT SHALL BE DEEMED ADEQUATE. IF THE RUDDER HEAD IS MORTISED FOR THE TILLER, THE CHEEK PIECES SHALL BE NOT LESS THAN $\frac{3}{8}$ " THICK. THE RUDDER MAY BE STREAMLINED BELOW THE LOWER EDGE OF THE CHEEK PIECES BUT MUST HAVE A MINIMUM RADIUS OF $\frac{1}{8}$ " ALONG THE LEADING EDGE AND A MINIMUM OF $\frac{3}{32}$ " ALONG THE TRAILING EDGE. THE RUDDER WITH TILLER ATTACHED SHALL FLOAT. METAL CHEEK PIECES NOT LESS THAN $\frac{1}{16}$ " IN THICKNESS MAY BE SUBSTITUTED FOR WOOD.

18. *Skeg*:—The skeg shall be fitted as shown on the plans; sided dimensions not less than $\frac{3}{4}$ ", 4" deep at the end of the keel, 1" deep at station 7, terminating $3\frac{1}{2}$ " forward of station 7. The skeg shall not project beyond the finished after face of the transom.

Maximum sided dimension shall be no greater than $\frac{7}{8}$ ".

19. *Floorboards*:—Floorboards having a total area of 16 square feet (excluding slots between boards), capable of supporting a 150 pound person standing midway between frames, shall be fitted. Solid $\frac{1}{4}$ " plywood floorboards having an area equal to 16 square feet may be used if desired.

20. *Decks*:—No decking, washboards or coamings are permitted. A fabric spray hood forward of Frame 2 may be fitted. NO MORE THAN TWO $1"x\frac{1}{4}"$ BATTENS MAY BE USED FOR THE SPRAY HOOD.

21. *Spars*:—Spars shall be solid except that a groove of not more than $\frac{1}{2}$ " diameter for luff of the sail may be incorporated in the mast. The maximum fore and aft dimensions of mast, including the sail slot, shall not exceed $3\frac{1}{2}$ ". The minimum thickness at the stays and below shall not be less than $1\frac{3}{4}$ ". The top of the mast shall not extend beyond a height of 19' above the top of the keel. Mast may be joined or in one piece. BOOM SHALL BE SO CONSTRUCTED THAT WITHOUT RIGGING, JAWS OR HARDWARE IT CAN BE PASSED THROUGH A 2" SQUARE HOLE. Spars shall be plainly marked with contrasting bands $\frac{1}{2}$ " wide; as follows: Mast 16' between the inner edges of the bands, boom, 8'8" from center of groove or the face of the sail track to inner edge of band. Mast may be rotated about its longitudinal axis while underway but devices for otherwise moving or bending it will not be permitted. If desired the mast may be tapered from the $2\frac{1}{4}$ " at 17" above the butt end to 1" at the butt end. Spars in their normal state must be straight, no steam bending of either mast or boom is allowed.

22. *Rigging*:—Standing rigging shall consist of two shrouds and a head stay which shall not be less than $\frac{3}{32}$ " in diameter. A boom vang to hold the boom down is permitted provided a stop is placed on the mast so that the foot of the sail cannot be pulled past the limit band. A vang must be rigged so that it does not prevent a jibe without re-rigging. Halyard shall not be led so as to support the mast. No jumper stays or spreaders are allowed.

23. *Sails*:—The class shall carry a loose-footed sail, which when set must not encroach upon the limit marks on the spars and which conforms with the limiting dimensions shown on the Official Sail Plan. The sail may be fitted with a headboard not over $4\frac{1}{2}$ " wide, the board itself to be measured perpendicular to the luff. No stiffening material other than sail cloth, canvas or so leather may be used in the tack or clew. Cringles, rings or grommets in tack or clew shall not exceed $1\frac{3}{4}$ " in outside diameter. The sail shall be provided with no more than three battens, spaced evenly along the leach, and one batten in the foot. Maximum length of the battens shall be as follows: upper 18' center 24", lower 21" and foot 12". The Executive Committee reserves the right to use the sail as the area upon which honor awards shall be displayed and has designated certain areas of the sail, shown on the plates published herewith, to be used for this purpose. The maximum dimensions of the sail are to be as follows: Luff 16'-0", foot 8'-8", leach 16'-6", roach on foot 6". The draft (roach on luff) at the mid-point of the luff and the mid-point of the leach, shall not exceed 66". Any sail that, when properly stretched, will fit completely within this described area shall be deemed legal. In order to measure the midgirth dimension the following procedure is to be used: Fold the head of the sail down to the tack and then to the clew to establish the mid-point of the luff and leach. The sail is then laid out flat and the head tack and clew securely held (recommend using stakes or ice picks). The mid-points shall be pulled taut by hand as required to take out the wrinkles and give a flat measurement across the mid-girth. Sails must be perfectly dry when measured. The sails shall be of white material with black insignia and numbers shall be as indicated on the sail plan. Sail material shall be limited for a two year period ending evening of the Annual Meeting in 1956 to the following specific materials: Cotton (domestic or foreign), Nylon, Orlon (Acrylic Fiber), Dacron, (Polyester Fiber). The sailmaker shall be required to mark plainly the date of purchase on the sail.

24. The Official Plans shall form a part of these specifications. Alterations of these plans may be made by the Executive Committee, as provided in Article XI of these By-Laws.

25. All boats, the framing of which was begun on or after 1 January 1956, must comply with all specifications as published herein. Boats, the framing of which was begun before 1 January 1956 may comply with these specifications or the specifications of record on 1 January 1955, provided they also comply by 1 January 1956 with the limits as set forth in sections 2,9,11,12, and 22 as published herein.

ARTICLE XI.

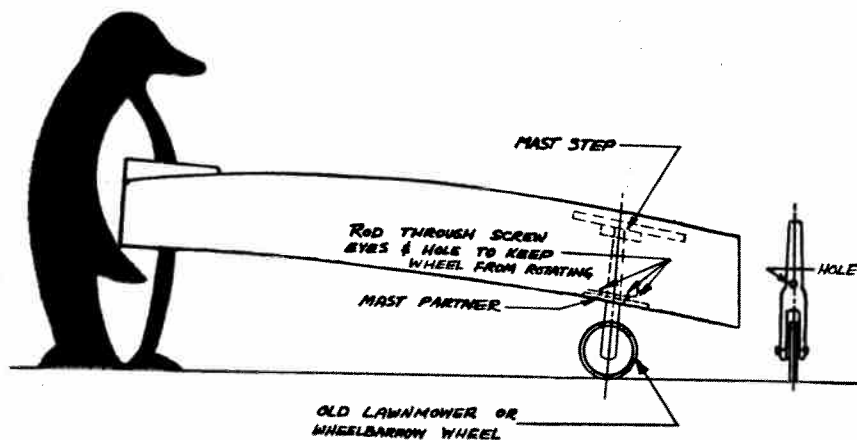
These By-Laws may be amended, at any time, by a two-thirds vote of the Executive Committee.

Note: With reference to Article X, Sec. 2, the minimum weight clause; it is recommended that the hull be weighed at a coal yard or on other certified scales. If these are not available, the boat can be weighed by turning upside down and placing stem on one bathroom scale and transom on another. Just total the two readings.

The designer of the Penguin plans has assigned all rights and title to the International Penguin Class Dinghy Association, and plans may be purchased only through the Bus. Sec'y of the Association. The fee for plans is \$8.00 payable to the International Penguin Class Dinghy Association. The plans consist of lithographed prints of the lines and offsets, construction drawings, sail plan, detailed spar drawing, full size templates for all frames, stem and stern knees and centerboard trunklogs.

The International Penguin Class Dinghy Association has no interest in, or affiliation with, any professional yacht builder, and the plans are available to amateur and professional builders alike.

The Association will refuse to issue a number or grant a measurement certificate to any boat on which the royalty has not been paid or which was not built from a recorded set of Penguin plans. A royalty or number tax of \$5 must be paid by the builder to the Association on each additional boat built from a set of plans. No boat may properly be sold, or entered in any race as a Penguin, or at any time display the Penguin Emblem upon its sails or otherwise, unless the number tax or royalty on such boat has been paid to the Association by the builder or owner thereof. For the benefit of owners, and prospective owners, the Association proposes to take whatever steps that may be necessary to protect the name and Emblem from any unauthorized use.



PENGUIN RACING RULES

I. General

It is the intention of the Penguin Class Dinghy Association to adhere strictly to North American Yacht Racing Union "Definitions" and "Right of Way Rules," as set forth by Section III and IV. Other parts of the Rules eliminate all reference to details which are not applicable to dinghy racing. Penalties for infractions have been made flexible in order to be fair and just, as it is not believed that all infractions of a minor nature justify disqualification. The Rules will be strictly applied, and it is hoped that in such application the results desired, namely, greater pleasure and a high degree of sportsmanship, will be obtained.

I. General.

1. The Race Committee shall have complete charge and direction of the running of the races. All questions of interpretations of the rules shall be made by it and all penalties resulting from infractions of the rules shall be assigned by it. Decisions shall be based on these rules and shall be made in such a manner to obtain fairness and justice to all contestants and encourage and reward superior sailing skill.

2. The Race Committee may refuse any entry made after the time and date specified for the time of closing entries and if in their judgment it is to the best interests of the sport, cancel or reject any entry at any time.

3. Every entry shall be furnished or have made available to him at the time of registry, written instructions as to the conditions of the race.

4. The Race Committee shall have authority to postpone the start of any race. Postponements may be for any time interval and shall continue until five minutes after the lowering of the postponement signal. The signal for postponement shall be code flag P.

5. If in the opinion of the Race Committee, the weather or sea conditions are such that the race no longer represents a test of sailing skill, they may shorten the course or cancel the race. The signal for cancellation will be code flag C. The signal for shortening course will be code flag S.

II. Marks of the Course and Signals

1. The course shall be marked with buoys, boats or other marks as described in the instructions.

2. The start and finish line shall be indicated by a white flag on the Committee boat and a white flag on a stake boat.

3. If a mark is damaged or shifted, the Race Committee shall attempt to re-establish the mark or replace it with a boat displaying code flag M. If the mark cannot be re-established, or an alternate mark cannot be established, the Race Committee has the power to cancel the race.

4. Attention shall be called to all signals by means of a gun, horn or whistle. Visual signals shall be struck 30 seconds before next signal.

5. Starting signals shall be in accordance with the following table.

Signal	Visual	Audible	Time To Start
Warning	White Shape	2 Guns	10 min.
Preparatory	Blue Shape	1 Gun	5 min.
Start	Red Shape	1 Gun	Start

III. Definitions

Terms defined below are capitalized in the text of the Rules.

STARTING AND FINISHING—A yacht Starts when, after her start signal, her mainmast (or such other point as may be designated in the instructions) first crosses the starting line in the direction of the first Mark; and Finishes when it crosses the finish line from the direction of the last Mark. If the committee boat, either from necessity or error, is anchored on the side of a starting or finish Mark opposite to that indicated in the instructions, a yacht can start or finish properly only if she complies with the preceding sentence.

LEEWARD AND WINDWARD—To Leeward, on the side on which a yacht is, or, if luffing head to wind was, carrying her main boom; to Windward, on the other side. These terms, when applied to yachts, apply only to yachts on the same tack. A yacht to Leeward or to Windward may be either Clear Ahead, Overlapping or Clear Astern, but the terms Windward Yacht or Leeward Yacht apply only to yachts that Overlap.



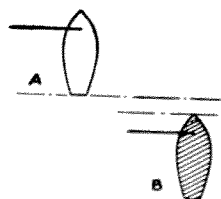
"A" is windward yacht; "B" is leeward yacht

ON A TACK—A yacht is On a Tack except when she is Tacking or Jibing. A yacht is on the tack (starboard or port) corresponding to her Windward Side.



"A" is on port tack; "B" is on starboard tack

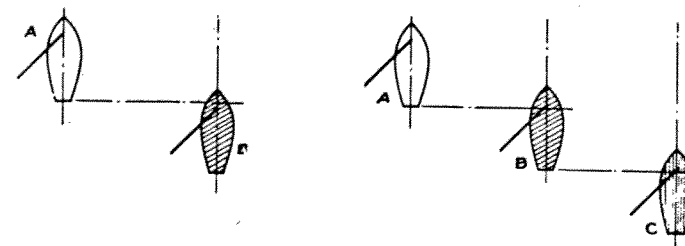
CLEAR ASTERN, CLEAR AHEAD—A yacht is Clear Astern of another when her hull, spars and sails are aft of an imaginary line projected abeam from the aftermost point of the other's hull and spars. The other yacht is Clear Ahead.



"A" is clear ahead

"B" is clear astern

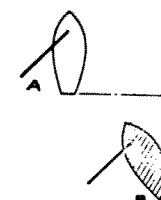
OVERLAP—When neither yacht is Clear Astern, the yachts Overlap. Two yachts also overlap when an intervening yacht overlaps both of them.



"B" has overlap on "A"

"C" has overlap on "B" and also on "A"

OVERTAKING—A yacht is Overtaking another when she is overhauling her from Clear Astern regardless of their respective courses. Overtaking terminates when an Overlap is established.



"B" is overtaking "A"

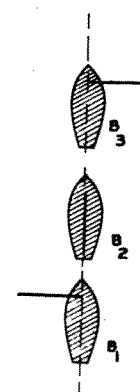
TACKING—A yacht is Tacking from the moment she is beyond head to wind until she bears away: if beating to windward, to a full and by course; if not beating to windward, to the course on which her mainsail fills.

JIBING—A yacht is Jibing from the moment when, with the wind aft, her main boom crosses her center line until her mainsail fills on the other tack.



"A" is tacking from pos. 2 to pos. 3

"B" is "jibing" from pos. 2 to pos. 3



LUFFING AND BEARING AWAY—Luffing, altering course towards the wind. Bearing Away, altering course away from the wind. But a yacht is not Luffing or Bearing Away "in such manner as to balk or mislead" (Rule 5) if, when beating to windward, she alters her course to conform to a change in the strength or direction of the wind.

NORMAL COURSE—A Normal Course is any course a yacht might sail, after the start signal and in the absence of the other yacht, to finish as quickly as possible. The course sailed before beginning to luff another yacht or bear away is presumably a Normal Course.

MARK—The Marks for each yacht are the objects that the instructions require her to pass between or on a specified side. A Mark has a required side for a yacht only when she Starts, or when it begins, bounds or ends the leg of the course on which she is racing, and it ceases to have a required side for a yacht once she has properly passed or rounded the next Mark and left it astern; or in the case of a finish Mark, once she has Finished. Every ordinary part of a Mark (including a flag, flag pole, boom or hoisted boat) is part of it but not ground tackle or an accompanying government marker buoy, it and the shoal or object that it bounds.

OBSTRUCTION—Obstructions are craft underway (including another yacht racing having the right of way), craft at anchor, craft aground, wrecks, the shore, piers, structures, fish weirs, fish traps, and shoals over which the yacht in question may not be safely piloted. A buoy is not of itself an Obstruction, but it is part of one when the yacht in question cannot safely pass between it and the shoal or object that it bounds.

IV. Right of Way Rules

The Rules of Sec. IV apply between yachts intending to race from the time they begin to sail about in the vicinity of the start until they have left the vicinity of the course either between races or after their racing is over for the day. At other times (and at all times between other yachts) the Government Right of Way Rules apply. For the purpose of penalties, the Rules of Sec. IV apply from yacht's preparatory signal until she has finished and cleared the finish line.



Rules That Apply At All Times

1. ON OPPOSITE TACKS, FUNDAMENTAL RULE—A port tack yacht shall keep clear of a starboard tack yacht.



"A," on starboard tack, has right of way

2. ON SAME TACK, FUNDAMENTAL RULE—A Windward Yacht shall keep clear of a Leeward Yacht. An overtaking yacht shall keep clear and, when establishing an Overlap to Leeward, shall allow the Windward Yacht ample room and opportunity to fulfill her newly acquired obligation to keep clear.

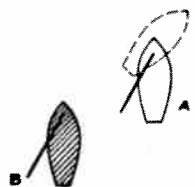


"A," leeward yacht, has right of way

"A," clear ahead, has right of way

"B" has overtaken "A," established overlap and has right of way, but must allow "A" room to keep clear and room to luff.

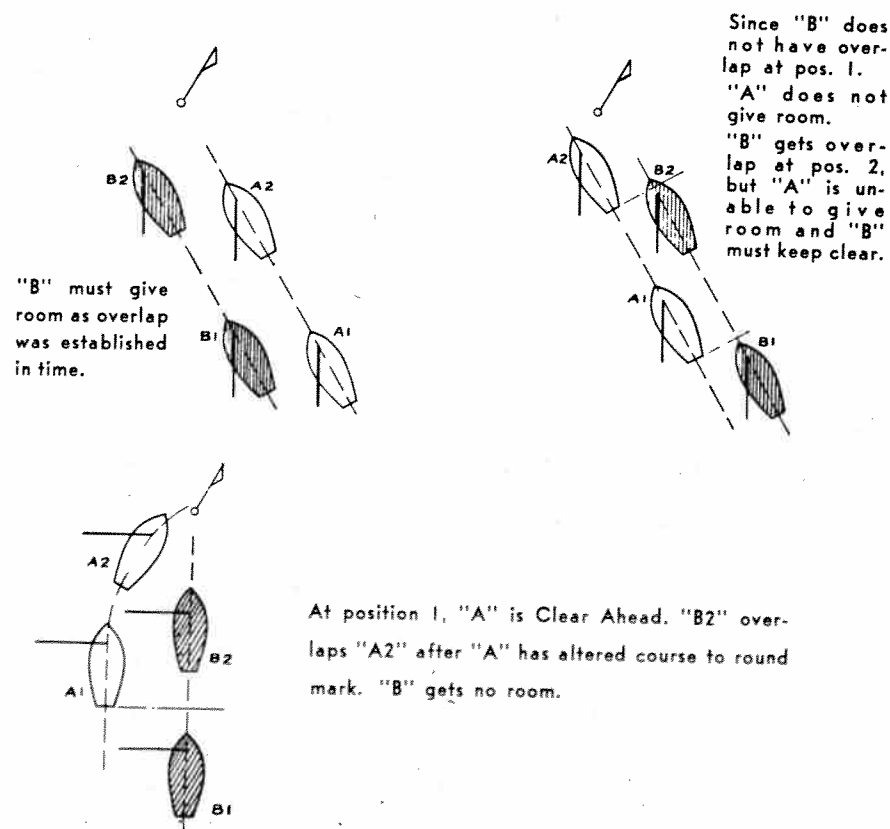
Note: "B" must allow for swing of "A's" stern.

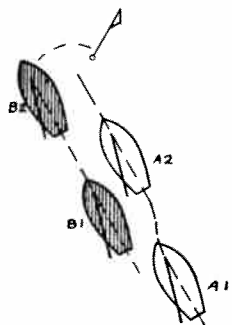


3. ON SAME OR OPPOSITE TACKS, PASSING MARKS AND OBSTRUCTIONS—When yachts on the same tack that are not subject to the Anti-Barging Rule are about to pass on the same side of a Mark or Obstruction, or when yachts on opposite tacks are about to pass on the required side of a Mark terminating a down wind leg of the course, an outside yacht shall give each yacht Overlapping her on the inside room to pass or round it. But a yacht shall keep clear in anticipation of and during the passing or rounding maneuver if she is Clear Astern when the foremost point of the other yacht is abreast of it, or if she established an inside Overlap on the other yacht too late to enable the other yacht to give the required room.

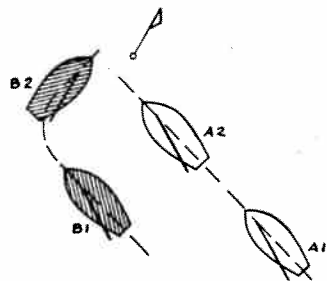
A yacht Clear Ahead is under no obligation to give room before an Overlap is established. In case of doubt, it will be presumed that an inside Overlap was improperly established.

When this rule applies, it overrides all rules except Rule 9 but a yacht Clear Ahead cannot claim the right to tack under this rule: if she is completing the rounding of a leeward Mark and the next leg is a beat; or if the yachts are beating to windward, and are about to tack for or around a Mark or Obstruction, and the yacht Clear Astern is to Windward. In either case the yacht Clear Ahead in Tacking is subject to Rule 6.





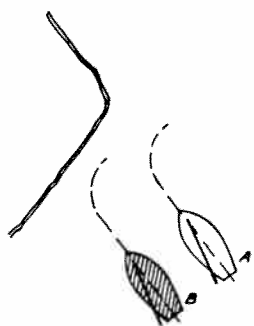
"B" cannot tack around mark if she interferes with "A" while so doing.



"B" cannot tack for mark as "A" has right of way due to her starboard tack.

4. ON SAME TACK, HAILING FOR ROOM TO TACK AT OBSTRUCTIONS—When two yachts are standing close-hauled on the same tack towards an Obstruction, and one is to Leeward of and either Clear Ahead of or Overlapping the other, the helmsman of the one to Leeward, if he cannot fetch the Obstruction and if he cannot tack without colliding with the yacht to Windward, may hail for room to tack when safe piloting requires him to take action to clear it. After such hail, the yacht to Windward shall at once allow the yacht to Leeward room to tack, and the yacht to Leeward shall tack as soon as she has room to do so. If, however, the Obstruction is a Mark or a right of way yacht on the opposite tack, the yacht to Leeward may not hail for room to tack if the yacht to Windward can fetch the Obstruction.

If a hail to tack is improper for any reason, the yacht to windward may hold her course. If she decides to do so, she shall promptly so advise the yacht to Leeward. When there is doubt, a hail to tack shall be deemed proper.

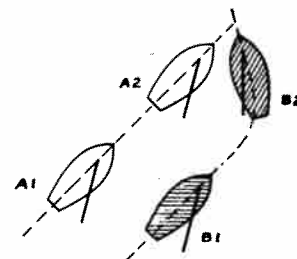


"B" may hail for "A" to come about and "A" must respond immediately.

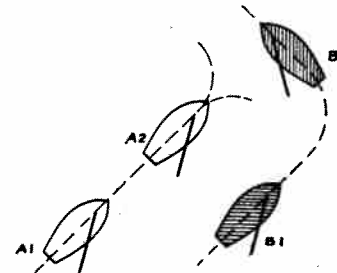
5. ON SAME OF OPPOSITE TACKS, ALTERING COURSE—A right of way yacht shall never alter course when the other yacht is unable to respond owing to her position, or in such manner as to balk or mislead her when she is in the act of keeping clear.

6. TACKING AND JIBING—A yacht while Tacking or Jibing shall keep clear of a yacht On a Tack. When a yacht tacks or jibes into a position that will subsequently give her right of way, she must do so far enough away from a yacht On a Tack to enable the latter to hold her course until the tack or jibe is completed and thereafter to keep clear. When there is doubt it shall be presumed that a yacht tacked or jibed too close to another yacht.

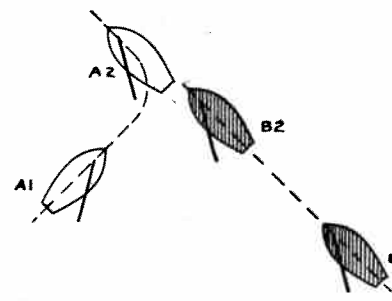
If two yachts are Tacking or Jibing at the same time, the one on the other's port side shall keep clear.



"B2," who is tacking must keep clear of "A2," who is on a tack.



"B2" must be far enough ahead so that when she fills on new tack, "A2" will have time to keep clear by tacking or bearing off.

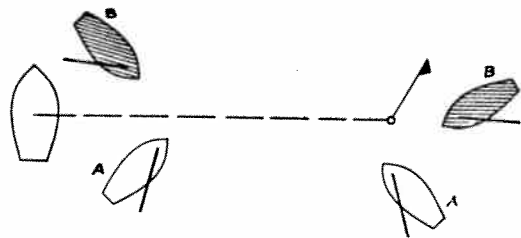


"B2" must have time to keep clear after "A2" fills away on new tack.

Rules That Apply Only At the Start

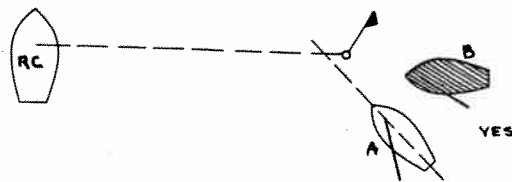
7. ON SAME TACK, ALTERING COURSE—Before Starting, a yacht Clear Ahead or a Leeward Yacht, when altering her course affects another yacht, may alter course but only slowly, and a Leeward Yacht may Luff only whenever the helmsman of the Windward Yacht (sighting abeam from his normal station) is aft of the mainmast of the Leeward Yacht. The last paragraph of Rule 10 also applies.

8. ON SAME TACK, ANTI-BARGING RULE—When approaching the starting line to Start, a Leeward Yacht is under no obligation to give any Windward Yacht room to pass to Leeward of a starting Mark surrounded by navigable water; but a Leeward Yacht shall not, after the start signal, deprive a Windward Yacht of room at such Mark either by heading above the first Mark or by luffing above close-hauled.

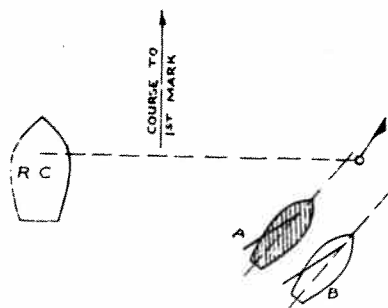


"B" is on wrong side of line at starting signal and must keep clear of "A" who is on the right side of the line or its extension, regardless of any other right of way rules.

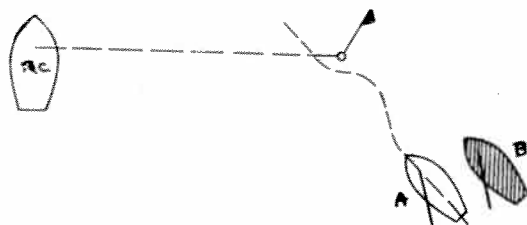
9. ON SAME OR OPPOSITE TACKS, ON WRONG SIDE OF STARTING LINE AT START SIGNAL—A yacht on the wrong side of the starting line or of its extensions when her start signal is made, shall, until she has Started, keep clear of all yachts that were on the right side of such line when their start signal was made. When this rule applies, it overrides all other rules.



After the start: "A" can maintain her course but cannot sail above the first mark or luff above close hauled.



"A" cannot head above course to first mark to force "B" outside of starting mark.



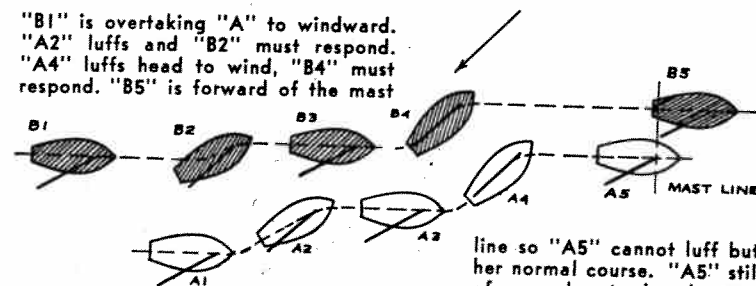
No, if original course was close hauled.

Altering Course After Starting

10. ON SAME TACK, LUFFING—After starting a yacht may Luff a yacht Clear Astern or a Windward Yacht as she pleases and head to wind if she pleases, until the helmsman of the Windward Yacht (when sighting abeam from his normal station and sailing no higher than the Leeward Yacht) comes abreast of the mainmast of the Leeward Yacht. Thereafter, or if he (when sighting abeam from his normal station) was forward of the mainmast of the Leeward Yacht when the Overlap began, the Leeward Yacht may not sail above her Normal Course while that Overlap continues to exist.

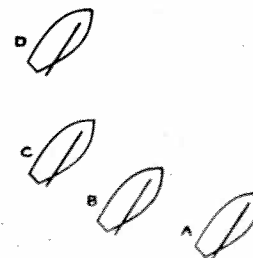
For the purpose of this rule: An Overlap does not begin or continue to exist unless the yachts are clearly within two overall lengths of the longer yacht; and an Overlap that exists between two yachts when they Start, or when one or both of them completes a tack or jibe shall be regarded as beginning at that time.

When there is doubt, the Leeward Yacht may assume that she has the right to Luff unless the helmsman of the Windward Yacht has hailed "Mast Abeam," or words to that effect. The Leeward Yacht must be governed by such hail, and, if she deems it improper, her only remedy is to protest. The Windward Yacht shall not cause a luff to be curtailed because of her proximity to the Leeward Yacht unless an Obstruction, a third yacht or other object restricts her ability to respond. A yacht shall not luff unless she has the right to luff all yachts that would be affected by her luff, in which case they shall all respond, even if an intervening yacht would not otherwise have the right to luff.



"B1" is overtaking "A" to windward. "A2" luffs and "B2" must respond. "A4" luffs head to wind, "B4" must respond. "B5" is forward of the mast

line so "A5" cannot luff but must sail her normal course. "A5" still has right of way due to her leeward position and "B5" must keep clear.



"A" has luffing rights over "B," "C" and "D" since they are all overtaking her to windward. "B," "C" and "D" must all respond to "A's" luff in spite of the fact that "C" has no luffing rights over "D" who is forward of the mast line.

11. ON SAME TACK, BEARING AWAY—When sailing on a free leg of the course, a yacht shall not sail below her Normal Course when she is clearly within three of her overall lengths of a Leeward Yacht or of a yacht Clear Astern that is steering a course to pass to Leeward.